

# DEKALB COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]  
[Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
DEKA01	J 777	Grindstone Creek Bridge	12-20' steel stringer 1931 H.C. Botsford
DEKA02	014000.3	Third Fork Bridge	1- 60' pinned Pratt half-hip pony truss c1905 Dildine Bridge Company [prob.]
DEKA03	026001.0	Bridge	1- 43' pinned Pratt bedstead 1901 Dildine Bridge Company
DEKA04	034001.5	Little Third Fork Bridge	1- 30' pinned Pratt half-hip pony truss c1905 Dildine Bridge Company [prob.]
DEKA05	036000.4	Little Third Fork Bridge	1- 60' pinned Pratt half-hip pony truss c1905 Dildine Bridge Company [prob.]
DEKA06	043000.5	Bridge	1- 30' pinned Pratt half-hip pony truss 1902 Dildine Bridge Company
DEKA07	044000.6	Third Fork Bridge	(replaced)
DEKA08	069001.6	Lost Creek Bridge	1- 40' pinned Pratt half-hip pony truss 1907 Dildine Bridge Company
DEKA09	074000.2	Lost Creek Bridge	1- 40' pinned Pratt pony truss 1895 Dildine Bridge Company [prob.]
DEKA10	078000.5	Lost Creek Bridge	(replaced)
DEKA11	081000.6	Lost Creek Bridge	1- 60' pinned Pratt half-hip pony truss 1907 Dildine Bridge Company
DEKA12	082000.1	Lost Creek Bridge	1- 60' pinned Pratt pony truss 1893 Dildine Bridge Company
DEKA13	083000.3	Lost Creek Bridge	1- 40' pinned Pratt pony truss c1900 Dildine Bridge Company [prob.]
DEKA14	108001.1	Lost Creek Bridge	(replaced)
*DEKA15	115000.1	Lost Creek Bridge	1- 40' pinned Pratt half-hip pony truss c1900 Dildine Bridge Company [prob.]
DEKA16	124000.9	Lost Creek Bridge	1- 60' pinned Pratt half-hip pony truss 1904 Dildine Bridge Company
DEKA17	129000.0	Lost Creek Bridge	(replaced)
*DEKA18	131002.5	Grindstone Creek Bridge	1- 25' pinned kingpost pony truss 1901 Dildine Bridge Company
*DEKA19	139000.3	Lost Creek Bridge	1- 60' pinned Pratt half-hip pony truss 1894 Dildine Bridge Company
DEKA20	160000.7	Big Muddy Creek Bridge	1- 50' pinned Pratt pony truss 1904 Dildine Bridge Company
DEKA21	160002.5	Big Muddy Creek Bridge	1- 37' pinned Pratt half-hip pony truss c1910 Dildine Bridge Company [prob.]
*DEKA22	177000.5	Grindstone Creek Bridge	1- 60' 2-angle Pratt pony truss c1925
*DEKA23	177000.8	Grindstone Creek Bridge	1-100' pinned Pratt through truss 1899 Dildine Bridge Company
*DEKA24	193001.1	Lost Creek Bridge	1- 80' pinned Pratt through truss 1894 Dildine Bridge Company

## DEKALB COUNTY

### INCLUDED (cont.):

DEKA25	195001.5	Branscombe Bridge	1- 60'	pinned Pratt pony truss
			1903	Dildine Bridge Company
DEKA26	196000.1	Lost Creek Bridge	1- 40'	pinned Pratt half-hip pony truss
			1898	Dildine Bridge Company
DEKA27	211001.1	Grindstone Creek Bridge	1- 64'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
DEKA28	215003.7	Grindstone Creek Bridge		(replaced)
DEKA29	215004.2	Grindstone Creek Bridge		(replaced)
DEKA30	220001.1	Lost Creek Bridge	1- 70'	pinned Pratt pony truss
			1900	Dildine Bridge Company
DEKA31	227000.7	Grindstone Creek Bridge		(replaced)
DEKA32	241000.9	Lost Creek Bridge	1- 60'	pinned Pratt pony truss
			1911	Dildine Bridge Company
DEKA33	260000.9	Bridge		(replaced)
DEKA34	267000.2	Castile Creek Bridge	1- 60'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
*DEKA35	278000.8	Lost Creek Bridge		(replaced)
*DEKA36	296000.8	Bridge		(replaced)
DEKA37	298000.1	Little Third Fork Bridge	1- 40'	pinned Pratt half-hip pony truss
			1895	Dildine Bridge Company
DEKA38	299000.3	Little Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1894	Dildine Bridge Company
DEKA39	304000.8	Little Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1901	Dildine Bridge Company
DEKA40	305000.6	Little Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1898	Dildine Bridge Company
DEKA41	312000.4	Third Fork Bridge		(replaced)
DEKA42	322001.1	Morgan Branch Bridge		(replaced)
DEKA43	322002.2	Little Third Fork Bridge		(replaced)
DEKA44	328001.3	Morgan Branch Bridge	1- 40'	pinned Pratt half-hip pony truss
			c1905	Dildine Bridge Company [prob.]
DEKA45	328001.9	Little Third Fork Bridge	1- 64'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
*DEKA46	330000.4	Bridge		(replaced)
DEKA47	336000.8	Third Fork Bridge	1- 60'	pinned Pratt half-hip pony truss
			1899	Dildine Bridge Company
DEKA48	338001.3	Bridge		(replaced)
DEKA49	342000.4	Third Fork Bridge		(replaced)
DEKA50	347000.5	Third Fork Bridge		(replaced)
DEKA51	356000.2	Little Third Fork Bridge	1- 70'	pinned Pratt pony truss
			c1910	Dildine Bridge Company [prob.]
DEKA52	376000.4	Jordan Creek Bridge	1- 35'	pinned Pratt half-hip pony truss
			1893	Dildine Bridge Company
DEKA53	380000.2	Bridge	1- 30'	pinned Pratt half-hip pony truss
			c1905	Dildine Bridge Company [prob.]

## DEKALB COUNTY

### INCLUDED (cont.):

DEKA54	391000.9	Castile Creek Bridge	1- 40'	pinned Pratt half-hip pony truss
			1895	Dildine Bridge Company
DEKA55	391001.1	Bover Bridge	1- 40'	pinned Pratt half-hip pony truss
			c1900	Dildine Bridge Company [prob.]
DEKA56	391001.7	Castile Creek Bridge	1- 30'	pinned Pratt half-hip pony truss
			1893	Dildine Bridge Company
DEKA57	422000.4	Lost Creek Bridge		(replaced)
DEKA58	453000.9	Wamsley Creek Bridge		(replaced)
*DEKA59	458001.6	Grindstone Creek Bridge		(replaced)
DEKA60	463000.7	Wamsley Creek Bridge		(replaced)
DEKA61	464000.1	Wamsley Creek Bridge		(replaced)
DEKA62	479000.2	Bridge		(replaced)
DEKA63	480001.2	Brushy Creek Bridge		(replaced)

### EXCLUDED:

#### Warren bedstead

005001.3	008000.9	008001.2	043000.3	052000.4	092000.6	124000.2
199000.6	248000.6	275000.5	354000.7	373000.2	423003.3	

#### Lattice bedstead

056003.0

#### Steel stringer

G 538R	J 674	J 675	J 776	J 777	L 165	S 419
T 641	X 138	X 139	X 140	X 473	013001.2	027001.8
028001.0	056001.5	059000.1	060001.1	060002.7	060002.8	064000.1
082000.5	085000.8	085001.9	088001.5	092R01.9	094000.9	099000.5
109000.3	112001.9	120002.3	131002.3	141001.8	156001.8	162000.4
163001.0	167000.7	168000.7	176000.2	177001.4	184000.1	205000.4
216000.3	226000.1	236000.3	255000.9	257000.5	259000.9	278001.1
280000.0	295002.1	316000.4	316002.0	317000.8	322000.3	331002.1
345001.8	346000.9	364001.0	364001.1	367001.3	375001.0	377001.5
397001.7	405000.9	436000.7	449000.2	451001.1	458003.6	460000.1
487000.1						

#### Steel girder

018000.6	071000.1	093000.4	101003.2	140000.1	178001.3	181000.1
239000.8						

#### Concrete girder

H 91R	J 605	J 700	443001.5
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#### Concrete box culvert

G 690R	J 247	P 25	328002.6
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## DEKALB COUNTY

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### SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	39	0	0	40
Excluded	19	84	0	0	103
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Total	20	123	0	0	143

# Grindstone Creek Bridge

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DEKA01

## GENERAL DATA

structure no.:	J 777	city/town:	1.5 miles east of Weatherby
county:	DeKalb	feature inters.:	Grindstone Creek
		cadastral grid:	S24, T59N, R30W
		highway route:	State Highway 6
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

## STRUCTURAL DATA

superstructure:	steel stringer		
substructure:	timber pile bent abutments, wingwalls and piers		
span number:	12	condition:	fair
span length:	20.0'	alterations:	none
total length:	243.0'	floor/decking :	concrete deck
roadway width:	20.0'	other features:	steel pipe guardrails

## HISTORICAL DATA

erection date:	1931
erection cost:	\$8483.96
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	H.C. Botsford
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 777; files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.
sign. rating:	42
evaluation:	NRHP non-eligible (typical example of common MSHD bridge configuration, distinguished somewhat by its multiplicity of spans)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Third Fork Bridge

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DEKA02

## GENERAL DATA

structure no.:	014000.3	city/town:	1.8 miles north of Union Star
county:	DeKalb	feature inters.:	Third Fork of Platte River
		cadastral grid:	S23, T60N, R33W
		highway route:	County Road 14
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent abutments with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: c1905  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 014000.3.

sign. rating: 35  
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 6 April 1993

# Bridge

DEKA03

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## GENERAL DATA

structure no.:	026001.0	city/town:	6.3 miles west of Fairport
county:	DeKalb	feature inters.:	branch of Lost Creek
		cadastral grid:	S14, T60N, R32W
		highway route:	County Road 26
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt bedstead		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	43.0'	alterations:	substructure replaced
total length:	43.0'	floor/decking :	concrete deck
roadway width:	13.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1901
erection cost:	\$300.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 026001.0; DeKalb County Court Record, Book 5: page 166 (4 February 1901), page 173 (7 February 1901) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	32
evaluation:	NRHP non-eligible (typically configured example of common structural type)

Inventoried by: Michelle Crow-Dolby    6 April 1993

# Little Third Fork Bridge

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DEKA04

## GENERAL DATA

structure no.:	034001.5	city/town:	5.3 miles east of Union Star
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S21/28, T60N, R32W
		highway route:	County Road 34
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss  
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	30.0'	alterations:	substructure replaced
total length:	30.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: c1905  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 034001.5.

sign. rating: 27  
evaluation: NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby 6 April 1993



# Little Third Fork Bridge

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DEKA05

## GENERAL DATA

structure no.:	036000.4	city/town:	5.6 miles east of Union Star
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S27/28, T60N, R32W
		highway route:	County Road 36
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 036000.4.
sign. rating:	35
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Bridge

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DEKA06

## GENERAL DATA

structure no.:	043000.5	city/town:	2.1 miles south of Union Star
county:	DeKalb	feature inters.:	branch of Third Fork of Platte River
		cadastral grid:	S3/10, T59N, R33W
		highway route:	County Road 43
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	30.0'	alterations:	none
total length:	30.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1902
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 043000.5; DeKalb County Warrant Register, Bridges: Dildine (1902) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	32
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

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DEKA08

## GENERAL DATA

structure no.:	069001.6	city/town:	4.9 miles west of Fairport
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S25, T60N, R32W
		highway route:	County Road 69
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	54.0'	floor/decking :	timber deck over timber stringers
roadway width:	11.9'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1907
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 069001.6; DeKalb County Court Record, Book 5: page 603 (9 April 1908) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	37
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

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DEKA09

## GENERAL DATA

structure no.:	074000.2	city/town:	2.2 miles north of Winslow
county:	DeKalb / Gentry	feature inters.:	Lost Creek
		cadastral grid:	S1/12, T60/61N, R32W
		highway route:	County Road 74
		highway distr.:	1
		current owner:	DeKalb County / Gentry County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	43.0'	floor/decking :	timber deck
roadway width:	15.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1895
erection cost:	\$300.00 (contract amount, divided equally between DeKalb and Gentry Counties)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 074000.2; Gentry County Court Record, Book I, page 233 (3 September 1895) - located at Gentry County Courthouse, Albany MO.
sign. rating:	34
evaluation:	NRHP non-eligible (typically configured example of common structural type)

Inventoried by: Clayton B. Fraser    6 April 1993

# Lost Creek Bridge

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DEKA11

## GENERAL DATA

structure no.:	081000.6	city/town:	5.8 miles north of Amity
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S1/12, T59N, R32W
		highway route:	County Road 81
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.0'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1907
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 081000.6; DeKalb County Court Record, Book 5: page 580 (7 October 1907), page 581 (8 October 1907) - located at the Dekalb County Courthouse, Maysville MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

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DEKA12

## GENERAL DATA

structure no.:	082000.1	city/town:	5.1 miles north of Amity
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S7/12, T59N, R31/32W
		highway route:	County Road 82
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: wrought iron or steel, 3-panel, pin-connected Pratt pony truss  
substructure: concrete-filled iron cylinder piers

span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	69.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1892-93  
erection cost: \$885.00  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 082000.1; DeKalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892) - located at the Dekalb County Courthouse, Maysville MO.

sign. rating: 48  
evaluation: NRHP possibly eligible (typically configured, well-preserved example of common structural type)

inventoried by: Clayton B. Fraser 6 April 1993

# Lost Creek Bridge

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DEKA13

## GENERAL DATA

structure no.:	083000.3	city/town:	5.0 miles north of Amity
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S7, T59N, R31W
		highway route:	County Road 83
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss with outriders  
substructure: unknown

span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	67.0'	floor/decking :	timber deck
roadway width:	13.5'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: c1900  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 083000.3; Warrant Register, Bridges: Dildine (1900) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 36  
evaluation: NRHP non-eligible (largely undocumented example of common structural type)

Inventoried by: Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

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DEKA15

## GENERAL DATA

structure no.:	115000.1	city/town:	2.4 miles north of Fairport
county:	DeKalb	feature inters.:	North Fork of Lost Creek
		cadastral grid:	S13/14, T60N, R31W
		highway route:	County Road 115
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt pony truss with laced endposts  
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	40.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.4'	other features:	upper chord and inclined end post: 2 channels with top and bottom lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with lacing; diagonal: 2 looped square eyebars; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date: c1900  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 115000.1; field inspection by Mitzi Rossillon, 8 September 1990.

sign. rating: 29  
evaluation: NRHP non-eligible (atypically configured example of common structural type, poorly documented and possibly moved to this location)

inventoried by: Michelle Crow-Dolby 6 April 1993



# Lost Creek Bridge

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DEKA16

## GENERAL DATA

structure no.:	124000.9	city/town:	2.7 miles south of Fairport
county:	DeKalb	feature inters.:	North Fork of Lost Creek
		cadastral grid:	S1/12, T59N, R31W
		highway route:	County Road 124
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent abutments with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1904  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 124000.9; DeKalb County Court Record, Book 5: page 328 (2 March 1903); Warrant Register, Bridges: Dildine (1904) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 40  
evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Grindstone Creek Bridge

DEKA18

## GENERAL DATA

structure no.:	131002.5	city/town:	3.2 miles northwest of Weatherby
county:	DeKalb	feature inters.:	branch of Grindstone Creek
		cadastral grid:	S8/9, T59N, R30W
		highway route:	County Road 131
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, pin-connected kingpost pony truss  
substructure: steel pile bent abutments embedded in concrete

span number:	1	condition:	fair
span length:	25.0'	alterations:	none
total length:	25.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.3'	other features:	inclined end post: I-beam; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with batten plates; lateral bracing: round rod with threaded ends; guardrail: 2 angles

## HISTORICAL DATA

erection date: 1900-01  
erection cost: \$300.00 (contract amount)  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : Carnegie Steel Company, Pittsburgh PA  
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 131002.5; DeKalb County Court Record, Book 5: page 125 (9 May 1900), page 166 (4 February 1901), page 197 (3 April 1901) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

sign. rating: 57  
evaluation: NRHP possibly eligible (well-preserved example of once-mainstay, now rare structural type)

inventoried by: Clayton B. Fraser 6 April 1993

# Lost Creek Bridge

DEKA19

## GENERAL DATA

structure no.:	139000.3	city/town:	1.8 miles east of Fairport
county:	DeKalb	feature inters.:	North Fork of Lost Creek
		cadastral grid:	S19, T60N, R30W
		highway route:	County Road 139
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss with laced ends; steel stringer approach span at east end		
substructure:	concrete abutments; steel pile bent pier (braced with angles)		
span number:	1	condition:	fair
span length:	60.0'	alterations:	original tubular-pier substructure replaced
total length:	76.0'	floor/decking:	timber deck over steel stringers
roadway width:	11.7'	other features:	upper chord: 2 channels with top and bottom lacing, continuous and batten plates; inclined end post: 2 channels with top and bottom lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diag- onal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I- beam, U-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date:	1894
erection cost:	\$871.00
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator:	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 139000.3; DeKalb County Court Record, Book 4: page 228 (8 August 1893), page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.
sign. rating:	51
evaluation:	NRHP possibly eligible (atypically configured, relatively early example of mainstay structural type)

inventoried by: Clayton B. Fraser    6 April 1993

# Big Muddy Creek Bridge

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DEKA20

## GENERAL DATA

structure no.:	160000.7	city/town:	2.3 miles west of Santa Rosa
county:	DeKalb	feature inters.:	Big Muddy Creek
		cadastral grid:	S22/27, T60N, R30W
		highway route:	County Road 160
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss  
substructure: steel pile bent abutments with timber wingwalls

span number:	1	condition:	fair
span length:	50.0'	alterations:	substructure replaced
total length:	50.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1904  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 160000.7; DeKalb County Court Record, Book 5: page 397 (4 May 1904) -located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 37  
evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Big Muddy Creek Bridge

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DEKA21

## GENERAL DATA

structure no.:	160002.5	city/town:	1.0 mile north of Santa Rosa
county:	DeKalb	feature inters.:	Big Muddy Creek
		cadastral grid:	S24/25, T60N, R30W
		highway route:	County Road 160
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent abutments with timber back- and wingwalls

span number:	1	condition:	fair
span length:	37.0'	alterations:	substructure replaced
total length:	37.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: c1910  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO (probable)  
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 160002.5.  
sign. rating: 30  
evaluation: NRHP non-eligible (typically configured, small-scale example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Grindstone Creek Bridge

DEKA22

## GENERAL DATA

structure no.:	177000.5	city/town:	2.7 miles south of Santa Rosa
county:	DeKalb	feature inters.:	Grindstone Creek
		cadastral grid:	S12, T59N, R30W
		highway route:	County Road 177
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 4-panel, rigid-connected, 2-angle Pratt pony truss  
substructure: concrete abutments with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	none
total length:	60.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.7'	other features:	upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 1 angle; lateral bracing: round rod with threaded ends; guardrail: 2 angles

## HISTORICAL DATA

erection date: c1925  
erection cost: unknown  
designer: unknown  
fabricator : Inland Steel Company, East Chicago IN  
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 177000.5; DeKalb County Court Record, Book 5: page 49 (5 June 1899); Warrant Register, Bridges: Dildine (1899) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

sign. rating: 34  
evaluation: NRHP non-eligible (uncommon structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 6 April 1993

# Grindstone Creek Bridge

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DEKA23

## GENERAL DATA

structure no.:	177000.8	city/town:	2.7 miles south of Santa Rosa
county:	DeKalb	feature inters.:	Grindstone Creek
		cadastral grid:	S12, T59N, R30W
		highway route:	County Road 177
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach span at each end		
substructure:	concrete abutments and concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	100.0'	alterations:	none
total length:	170.0'	floor/decking:	timber deck over steel stringers
roadway width:	11.5'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turn-buckle; lateral bracing: round rod with threaded ends; strut: lattice; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date:	1899
erection cost:	\$1750.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator:	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 177000.8; DeKalb County Court Record, Book 5: page 49 (5 June 1899); Warrant Register, Bridges: Dildine (1899) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.
sign. rating:	45
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser and Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

DEKA24

## GENERAL DATA

structure no.:	193001.1	city/town:	2.3 miles west of Weatherby
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S28/29, T59N, R30W
		highway route:	County Road 193
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt through truss, with laced ends; steel stringer approach span		
substructure:	concrete-filled steel cylinder piers; timber pile bent abutments with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	80.0'	alterations:	none
total length:	98.0'	floor/decking :	timber deck over steel stringers
roadway width:	11.7'	other features:	upper chord and inclined end post: 2 channels with top and bottom lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 looped square eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with turnbuckles (top) and round rods with threaded ends (bottom); strut: angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

## HISTORICAL DATA

erection date:	1894
erection cost:	\$1487.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 193001.1; DeKalb County Courthouse Record, Book 4: page 264 (8 March 1894), page 275 (8 May 1894) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.
sign. rating:	52
evaluation:	NRHP possibly eligible (well-preserved, relatively early example of main-stay structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993



# Branscombe Bridge

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DEKA25

## GENERAL DATA

structure no.:	195001.5	city/town:	3.7 miles northeast of Maysville
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S19/20, T59N, R30W
		highway route:	County Road 195
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	187.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1903
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 195001.5; DeKalb County Court Record, Book 5: page 150 (9 October 1900), page 306 (6 November 1902), page 320 (3 February 1903), page 328 (2 March 1903) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

DEKA26

## GENERAL DATA

structure no.:	196000.1	city/town:	3.2 miles northeast of Maysville
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S19, T59N, R30W
		highway route:	County Road 196
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	78.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1898
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 196000.1; DeKalb County Court Record, Book 4: page 429 (17 August 1897), page 434 (6 September 1897), page 467 (2 May 1898) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	39
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby    6 April 1993

# Grindstone Creek Bridge

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DEKA27

## GENERAL DATA

structure no.:	211001.1	city/town:	4.3 miles south of Weatherby
county:	DeKalb	feature inters.:	Grindstone Creek
		cadastral grid:	S10, T58N, R30W
		highway route:	County Road 211
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	64.0'	alterations:	substructure replaced
total length:	110.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 211001.1.
sign. rating:	34
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

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DEKA30

## GENERAL DATA

structure no.:	220001.1	city/town:	5.2 miles east of Maysville
county:	DeKalb	feature inters.:	Lost Creek
		cadastral grid:	S27/34, T59N, R30W
		highway route:	County Road 220
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	substructure replaced
total length:	137.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1900
erection cost:	\$1080.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 220001.1; DeKalb County Court Record, Book 5: page 74 (10 October 1899), page 110 (2 April 1900); Warrant Register, Bridges: Dildine (1900) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	40
evaluation:	NRHP non-eligible (typically configured example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Lost Creek Bridge

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DEKA32

## GENERAL DATA

structure no.:	241000.9	city/town:	1.4 miles southeast of Maysville
county:	DeKalb	feature inters.:	West Fork of Lost Creek
		cadastral grid:	S2, T58N, R31W
		highway route:	County Road 241
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1911
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor :	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 241000.9; DeKalb County Court Record, Book 6: page 99 (7 November 1910) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	34
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby    6 April 1993

# Castile Creek Bridge

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DEKA34

## GENERAL DATA

structure no.:	267000.2	city/town:	2.3 miles south of Amity
county:	DeKalb	feature inters.:	Castile Creek
		cadastral grid:	S13/24, T58N, R32W
		highway route:	County Road 267
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 267000.2.
sign. rating:	38
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Little Third Fork Bridge

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DEKA37

## GENERAL DATA

structure no.:	298000.1	city/town:	5.2 miles northwest of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S15, T59N, R32W
		highway route:	County Road 298
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	40.0'	floor/decking :	timber deck
roadway width:	11.2'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1895
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 298000.1; DeKalb County Court Record, Book 4: page 332 (6 May 1895) - located at the DeKalb County Courthouse, Maysville MO..
sign. rating:	46
evaluation:	NRHP possibly eligible (typically configured, small-scale example of mainstay structural type)

inventoried by: Clayton B. Fraser and Michelle Crow-Dolby    6 April 1993

# Little Third Fork Bridge

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DEKA38

## GENERAL DATA

structure no.:	299000.3	city/town:	5.2 miles northwest of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S15/16, T59N, R32W
		highway route:	County Road 299
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1893-94
erection cost:	\$784.00
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 299000.3; DeKalb County Court Record, Book 4: page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	49
evaluation:	NRHP possibly eligible (relatively early example of mainstay structural type)

Inventoried by: Clayton B. Fraser    6 April 1993



# Little Third Fork Bridge

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DEKA39

## GENERAL DATA

structure no.:	304000.8	city/town:	4.2 miles northwest of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S21/28, T59N, R32W
		highway route:	County Road 304
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent piers with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1901  
erection cost: \$700.00 (contract amount)  
designer: Dildine Bridge Company, Cameron MO (possible)  
fabricator : unknown  
contractor : Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 304000.8; DeKalb County Court Record, Book 5: page 166 (4 February 1901), page 173 (7 February 1901) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 40  
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Little Third Fork Bridge

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DEKA40

## GENERAL DATA

structure no.:	305000.6	city/town:	3.8 miles northwest of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S28, T59N, R32W
		highway route:	County Road 305
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent piers with timber wingwalls

span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1898  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 305000.6; DeKalb County Court Record, Book 4: page 429 (17 August 1897), page 434 (6 September 1897), page 467 (2 May 1898) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 42  
evaluation: NRHP non-eligible (typical example of common structural type)

Inventoried by: Clayton B. Fraser and Michelle Crow-Dolby 6 April 1993

# Morgan Branch Bridge

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DEKA44

## GENERAL DATA

structure no.:	328001.3	city/town:	3.0 miles northeast of Clarksdale
county:	DeKalb	feature inters.:	Morgan Branch
		cadastral grid:	S17, T58N, R32W
		highway route:	County Road 328
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent pier and abutments with timber wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	62.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: c1905  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO (probable)  
  
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 328001.3.  
  
sign. rating: 32  
evaluation: NRHP non-eligible (typical, small-scale example of common structural type, largely undocumented)

Inventoried by: Michelle Crow-Dolby    6 April 1993

# Little Third Fork Bridge

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DEKA45

## GENERAL DATA

structure no.:	328001.9	city/town:	4.2 miles west of Amity
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S8/17, T58N, R32W
		highway route:	County Road 328
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	64.0'	alterations:	substructure replaced
total length:	64.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 328001.9.
sign. rating:	34
evaluation:	NRHP non-eligible (typically configured example of common structural type, largely undocumented)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Third Fork Bridge

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DEKA47

## GENERAL DATA

structure no.:	336000.8	city/town:	4.8 miles northwest of Clarksdale
county:	DeKalb	feature inters.:	Third Fork of Platte River
		cadastral grid:	S3, T58N, R33W
		highway route:	County Road 336
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	substructure replaced; bridge closed
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1899
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 336000.8; DeKalb County Court Record, Book 5: page 8 (11 November 1898) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	42
evaluation:	NRHP non-eligible (typically configured example of common structural type)

Inventoried by: Clayton B. Fraser and Michelle Crow-Dolby    6 April 1993

# Little Third Fork Bridge

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DEKA51

## GENERAL DATA

structure no.:	356000.2	city/town:	immediately southeast of Clarksdale
county:	DeKalb	feature inters.:	Little Third Fork
		cadastral grid:	S25, T58N, R33W
		highway route:	County Road 356
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 5-panel, pin-connected Pratt pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	70.0'	alterations:	unknown
total length:	70.0'	floor/decking :	concrete deck
roadway width:	15.6'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 356000.2.
sign. rating:	38
evaluation:	NRHP non-eligible (typical example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby    6 April 1993

# Jordan Creek Bridge

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DEKA52

## GENERAL DATA

structure no.:	376000.4	city/town:	4.0 miles southwest of Clarksdale
county:	DeKalb	feature inters.:	branch of Jordan Creek
		cadastral grid:	S10, T57N, R33W
		highway route:	County Road 376
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss with laced end-posts		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	35.0'	alterations:	substructure replaced
total length:	35.0'	floor/decking :	timber deck
roadway width:	13.4'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1893
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 376000.4; DeKalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892), page 227 (7 August 1893) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	52
evaluation:	NRHP possibly eligible (relatively early, atypically configured example of mainstay structural type)

inventoried by: Clayton B. Fraser    6 April 1993

# Bridge

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DEKA53

## GENERAL DATA

structure no.:	380000.2	city/town:	4.0 miles southwest of Clarksdale
county:	DeKalb	feature inters.:	branch of Jordan Creek
		cadastral grid:	S10, T57N, R33W
		highway route:	County Road 380
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	30.0'	alterations:	substructure replaced
total length:	30.0'	floor/decking :	timber deck
roadway width:	11.6'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	c1905
erection cost:	unknown
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 380000.2.
sign. rating:	27
evaluation:	NRHP non-eligible (typically configured, small-scale poorly documented example of common structural type)

Inventoried by: Michelle Crow-Dolby    6 April 1993



# Castile Creek Bridge

DEKA54

## GENERAL DATA

structure no.:	391000.9	city/town:	3.0 miles northeast of Stewartsville
county:	DeKalb	feature inters.:	Castile Creek
		cadastral grid:	S3, T57N, R32W
		highway route:	County Road 391
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss, with steel stringer approach spans		
substructure:	steel pile bent pier and abutments with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced
total length:	65.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date:	1895
erection cost:	\$357.00 (contract amount)
designer:	Dildine Bridge Company, Cameron MO (probable)
fabricator :	unknown
contractor:	Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391000.9; DeKalb County Court Record, Book 4: page 354 (14 October 1895) - located at the DeKalb County Courthouse, Maysville MO.
sign. rating:	46
evaluation:	NRHP possibly eligible (relatively early example of mainstay structural type)

inventoried by: Clayton B. Fraser    6 April 1993

# Bover Bridge

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DEKA55

## GENERAL DATA

structure no.:	391001.1	city/town:	3.0 miles northeast of Stewartsville
county:	DeKalb	feature inters.:	branch of Castile Creek
		cadastral grid:	S2/35, T57N, R32W
		highway route:	County Road 391
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent abutments with timber wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	substructure replaced, 1910
total length:	40.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: c1900  
erection cost: unknown  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor : Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.1; DeKalb County Bridge Foreman's Reports (1910 - 1920): Bridge No. 1923, 30 March 1910 - 2 April 1910.

sign. rating: 27  
evaluation: NRHP non-eligible (typically configured example of common structural type, inadequately documented)

inventoried by: Michelle Crow-Dolby 6 April 1993

# Castille Creek Bridge

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DEKA56

## GENERAL DATA

structure no.:	391001.7	city/town:	3.4 miles northeast of Stewartville
county:	DeKalb	feature inters.:	branch of Castile Creek
		cadastral grid:	S2/35, T57N, R32W
		highway route:	County Road 391
		highway distr.:	1
		current owner:	DeKalb County

## STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss  
substructure: steel pile bent abutments with timber wingwalls

span number:	1	condition:	fair
span length:	30.0'	alterations:	substructure replaced
total length:	30.0'	floor/decking :	timber deck
roadway width:	11.3'	other features:	steel angle guardrails

## HISTORICAL DATA

erection date: 1893  
erection cost: \$230.00 (contract amount)  
designer: Dildine Bridge Company, Cameron MO (probable)  
fabricator : unknown  
contractor: Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.7; DeKalb County Court Record, Book 4: page 165 (26 September 1892), page 178 (6 December 1892) - located at the DeKalb County Courthouse, Maysville MO.

sign. rating: 46  
evaluation: NRHP possibly eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser    6 April 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

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**NAME(S) OF STRUCTURE**

Lost Creek Bridge  
MHTD: 082000.1

DEKA12

**DATE(S) OF CONSTRUCTION**

1892-93

**LOCATION**

County Road 82 over Lost Creek; S7/12, T59N, R31/32W  
5.1 miles north of Amity; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 48)

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**CONDITION**

fair

**OWNER**

DeKalb County

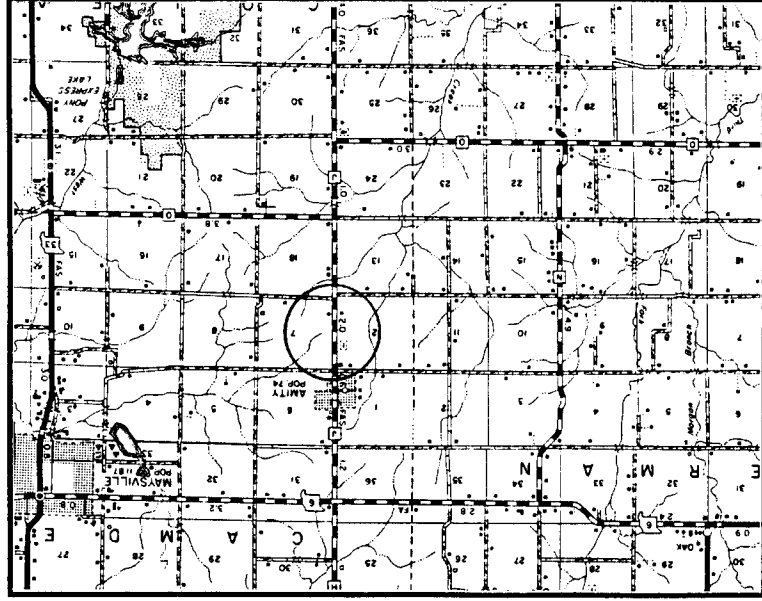
span number: 1  
span length: 60.0'  
total length: 69.0'  
roadway wdt.: 13.7'

superstructure: wrought iron or steel, 3-panel, pin-connected Pratt pony truss  
substructure: concrete-filled iron cylinder piers  
floor/decking: timber deck  
other features: steel angle guardrails

This medium-span truss spans Lost Creek on an unsurfaced county road some five miles north of Amity. The structure is comprised of a single pin-connected Pratt pony truss that rests on tubular piers. The Lost Creek Bridge was erected in 1892-93 by the Dildine Bridge Company of Cameron, Missouri for \$885.00. The structure is today distinguished by its relatively early construction date and its well-preserved condition. It typifies pinned truss construction in Missouri from the late 19th century.

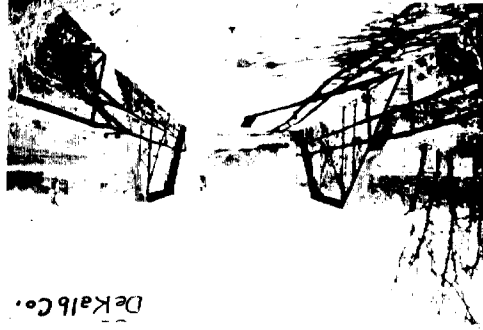
NAME(S) OF STRUCTURE  
Lost Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



Dekalb Co.



Dekalb Co.

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 082000.1; Dekalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892) - located at the Dekalb County Courthouse, Mayssville MO.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

6 April 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Grindstone Creek Bridge  
MHTD: 131002.5

DEKA18

**DATE(S) OF CONSTRUCTION**

1900-01

**LOCATION**

County Road 131 over branch of Grindstone Creek; S8/9, T59N, R30W  
3.2 miles northwest of Weatherby; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 57)

**CONDITION**

fair

**OWNER**

DeKalb County

span number:	1	superstructure:	steel, pin-connected kingpost pony truss
span length:	25.0'	substructure:	steel pile bent abutments embedded in concrete
total length:	25.0'	floor/decking:	timber deck over steel stringers
roadway wdt.:	13.3'	other features:	inclined end post: I-beam; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with batten plates; lateral bracing: round rod with threaded ends; guardrail: 2 angles

This small-scale truss bridge spans a branch of Grindstone Creek northwest of Weatherby, in east-central Dekalb County. Configured as a pin-connected kingpost pony truss, the single-span structure is supported by steel pile bent abutments embedded in concrete backwalls. The Grindstone Creek Bridge dates to May 1900, when the DeKalb County Court ordered the preparation of plans and specifications for a bridge at this location. After approving the engineer's drawings, court officials advertised for competitive construction bids. The Dildine Bridge Company of Cameron, Missouri, originally submitted a bid of \$336.00 but lowered the amount to \$300.00, and was subsequently awarded the contract. Dildine was contracted by the county almost exclusively for the erection of all its bridges between 1890 and the 1910s. Since its completion in 1901, the Grindstone Creek Bridge has continued to carry intermittent rural traffic with no major alterations of note.

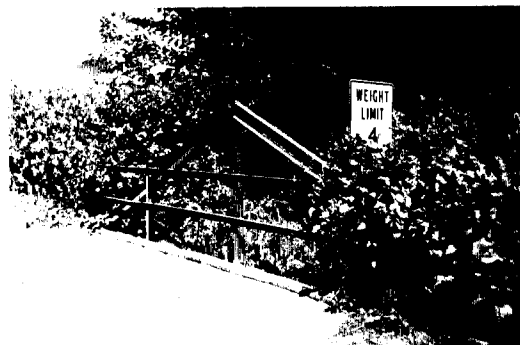
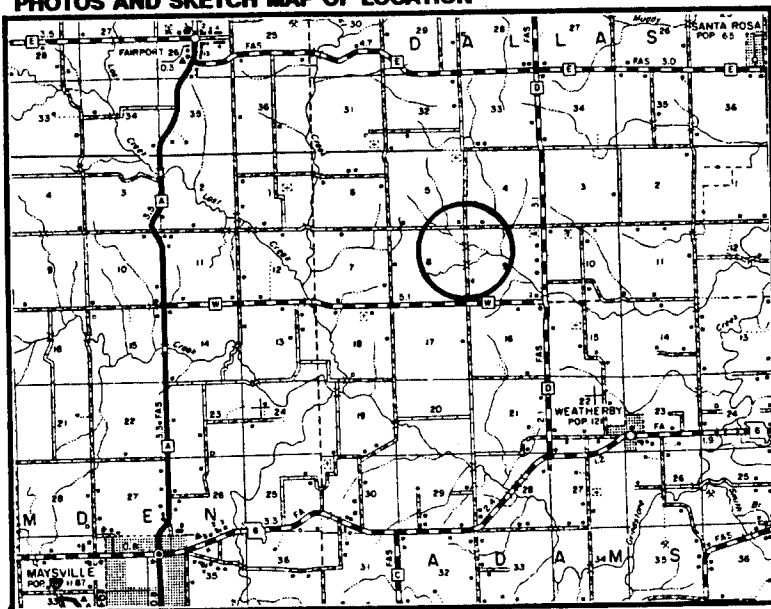
As one of northwestern Missouri's most prolific bridge builders, the Dildine Bridge Company maintained a roster of standard truss types, with a range of span lengths. Like most of Missouri's bridge builders of the time, Dildine relied heavily on pin-connected Pratt truss variants as his standard truss types. The short-span bridge that he erected for the Grindstone Creek crossing in 1901 was a kingpost pony truss, the most fundamental of the truss configurations. With its inclined endposts, straight lower chord and single vertical at mid-span, the kingpost formed a simple triangular web comprised of two equal panels. When the roadway was carried beneath the truss in a through configuration, the endposts acted in compression and the vertical and lower chord in tension.

The kingpost's origins are ancient and obscure. Its symmetrical triangular form lent itself naturally to timber roof framing, where the truss was first used in the Middle Ages. In 1570 Italian architect Andrea Palladio described a simple kingpost truss bridge in his general treatise on architecture, *I quattro libri della architettura*; he attributed its source to well-established antecedents. Using Palladio's work (translated into English in 1742) and their own empirical designs, early American carpenters constructed kingpost bridges at minor crossings throughout the East. The technology spread westward to Missouri with the pioneers in the late 18th and early 19th centuries. As a result, uncounted timber kingposts were built on the region's early roads. The truss form remained the same as its construction

evolved from the vernacular to the industrial in the 19th century, with the principle changes involving the materials used: timber, timber/iron, iron, steel. The kingpost as a structural type was generally limited to relatively short-span applications, however, and as steel beam bridges received widespread acceptance after the turn of the century, erection of kingpost trusses declined rapidly. The Grindstone Creek Bridge in DeKalb County is a well-preserved, rare example of this once-mainstay structural type.

**NAME(S) OF STRUCTURE**

Grindstone Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 131002.5; DeKalb County Court Record, Book 5: page 125 (9 May 1900), page 166 (4 February 1901), page 197 (3 April 1901) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 April 1993



# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Lost Creek Bridge  
MHTD: 139000.3

DEKA19

**DATE(S) OF CONSTRUCTION**

1894

**LOCATION**

County Road 139 over North Fork of Lost Creek; S19, T60N, R30W  
1.8 miles east of Fairport; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 51)

**CONDITION**

fair

**OWNER**

DeKalb County

span number: 1  
span length: 60.0'  
total length: 76.0'  
roadway wdt.: 11.7'

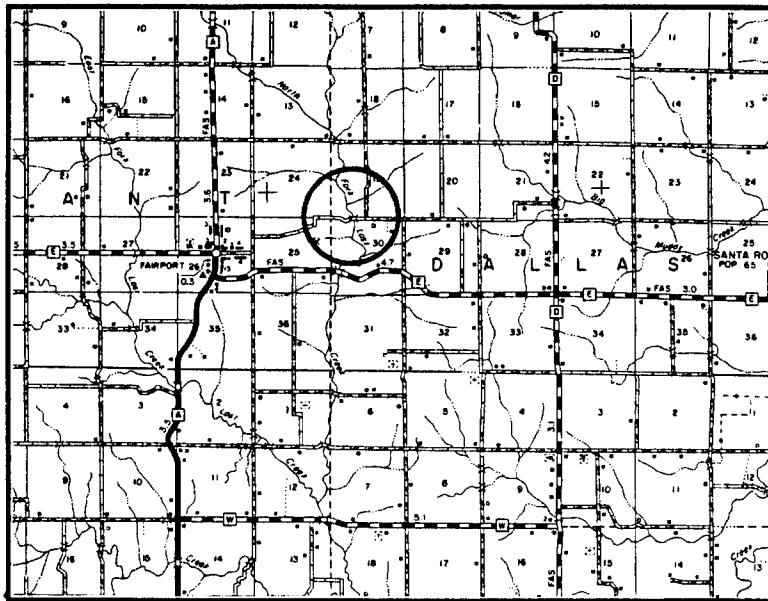
superstructure: steel, 3-panel, pin-connected Pratt half-hip pony truss with laced ends; steel stringer approach span at east end  
substructure: concrete abutments; steel pile bent pier (braced with angles)  
floor/decking: timber deck over steel stringers  
other features: upper chord: 2 channels with top and bottom lacing, continuous and batten plates; inclined end post: 2 channels with top and bottom lacing; lower chord: 2 looped rectangular eyebars; vertical: 4 angles with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

Crossing the North Fork of Lost Creek, this medium-span pony truss is located east of Fairport, in northeastern DeKalb County. The structure is comprised of a pinned Pratt half-hip pony truss, supported by steel pile bents and approached by a steel stringer span on its east end. The Lost Creek Bridge dates to August 1893. That month the DeKalb County Court instructed the county engineer to delineate plans for a bridge here. After approving the plans and specifications, the county court solicited competitive bids for the bridge's construction. Three months later the court hired the Dildine Bridge Company to fabricate and build the bridge for \$871.00. Dildine was contracted by the county almost exclusively for the erection of all its bridges between 1890 and the 1910s. The Lost Fork Bridge was originally erected on tubular iron piers; at some subsequent date a new substructure was built and the truss moved onto the new substructure. Since the move, the bridge continues to function in place today.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant that the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Lost Creek Bridge in DeKalb County is a typically configured example of this mainstay truss type. Built in 1894 by one of the state's most prolific bridge manufacturers, it is a well-preserved transportation-related resource.

**NAME(S) OF STRUCTURE**

Lost Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 139000.3; DeKalb County Court Record, Book 4: page 228 (8 August 1893), page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 April 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Grindstone Creek Bridge

MHTD: 177000.8

**LOCATION**

County Road 177 over Grindstone Creek; S12, T59N, R30W  
2.7 miles south of Santa Rosa; DeKalb County, Missouri

DEKA23

**DATE(S) OF CONSTRUCTION**

1899

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 45)

**CONDITION**

fair

**OWNER**

DeKalb County

span number: 1  
span length: 100.0'  
total length: 170.0'  
roadway wdt.: 11.5'

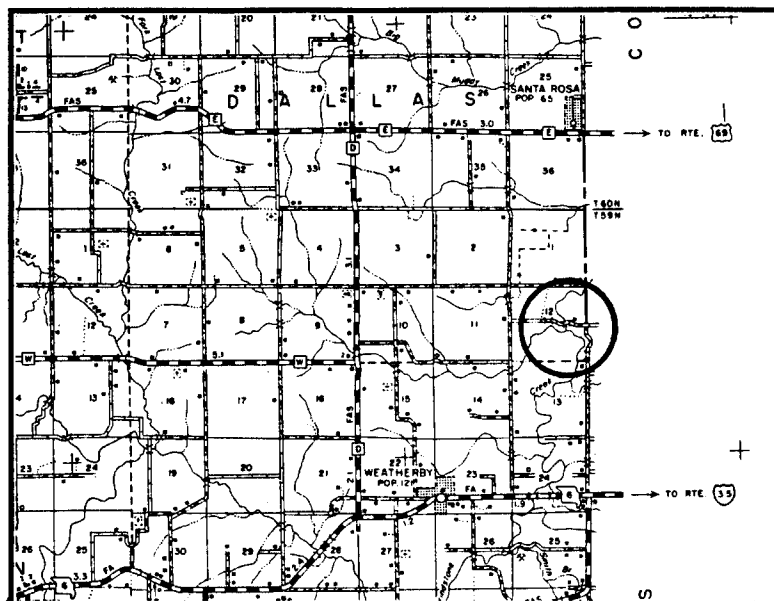
superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach span at each end  
substructure: concrete abutments and concrete-filled steel cylinder piers  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with threaded ends; strut: lattice; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

This structure carries County Road 177 across Grindstone Creek on the eastern edge of DeKalb County. Configured as a single-span, pinned Pratt through truss and approached on either side by steel stringers, the bridge rests on a concrete/steel substructure. The Grindstone Creek Bridge dates to 1898. In February of that year, the DeKalb County Court instructed the county engineer to delineate plans for a bridge at this crossing. After accepting the specifications, county officials let out the project for competitive bid. In June 1899, the Dildine Bridge Company of Cameron, Missouri, contracted with the county to provide and erect a 100-foot truss across the creek for the sum of \$1750.00. Dildine held a virtual bridge-building monopoly in the county from the 1890s to the 1910s. Continuing to function in place, the Grindstone Creek Bridge maintains a high degree of both historical and structural integrity.

In Missouri the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. Most of the structures erected during this period were based on standard plans developed either by the state highway department (after 1917) or by the individual bridge companies, such as the Dildine Bridge Company in Cameron. As a result, thousands of Pratts were built across the state, all very much alike in detailing, and today the Pratt truss constitutes the most populous group of through trusses. The Grindstone Creek Bridge is a typically configured example of the pin-connected Pratt through truss.

**NAME(S) OF STRUCTURE**

Grindstone Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 177000.8; DeKalb County Court Record, Book 5: page 49 (5 June 1899); Warrant Register, Bridges: Dildine (1899) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 April 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Lost Creek Bridge  
MHTD: 193001.1

DEKA24

**DATE(S) OF CONSTRUCTION**

1894

**LOCATION**

County Road 193 over Lost Creek; S28/29, T59N, R30W  
2.3 miles west of Weatherby; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 52)

**CONDITION**

fair

**OWNER**

DeKalb County

span number: 1  
span length: 80.0'  
total length: 98.0'  
roadway wdt.: 11.7'

superstructure: steel, 4-panel, pin-connected Pratt through truss, with laced ends; steel stringer approach span  
substructure: concrete-filled steel cylinder piers; timber pile bent abutments with timber back- and wingwalls  
floor/decking: timber deck over steel stringers  
other features: upper chord and inclined end post: 2 channels with top and bottom lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 looped square eyebars; counter: round rod with turnbuckle; lateral bracing: round rod with turnbuckles (top) and round rods with threaded ends (bottom); strut: angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

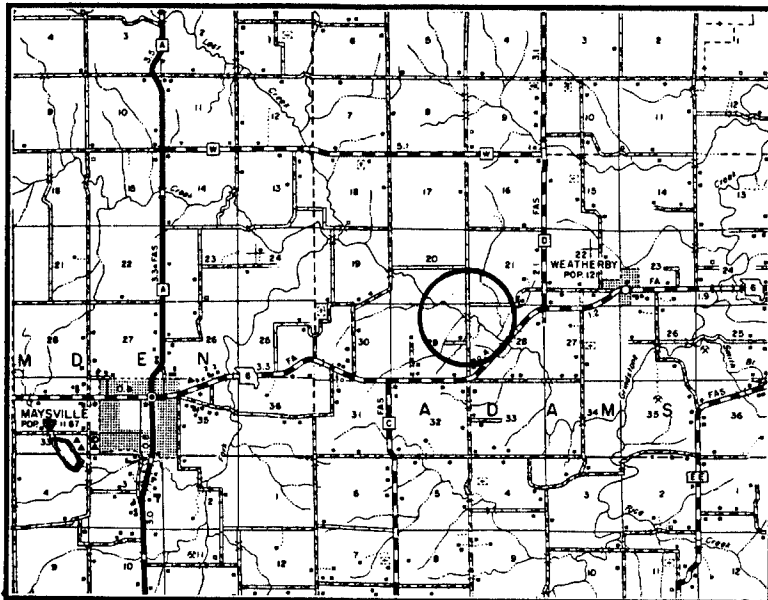
Located west of Weatherby in east-central DeKalb County, this pinned Pratt truss carries County Road 193 across Lost Creek. The structure is comprised of an 80-foot Pratt through truss, supported by concrete-filled cylinder piers and approached by a single steel stringer span on one side. The bridge dates to March 1894, when county officials instructed the county engineer to prepare plans and specifications for a permanent structure at this crossing. Two months later a contract to fabricate and erect the truss was let to the Dildine Bridge Company of Cameron, Missouri, for \$1487.00. Dildine held a virtual bridge-building monopoly in the county spanning from the 1890s to the 1910s. Since its completion, the Lost Creek Bridge continues to carry local traffic in its heavily wooded setting. The bridge retains a high degree structural integrity, with no major alterations of note.

In Missouri the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. Most of the structures erected during this period were based on standard plans developed either by the state highway department (after 1917) or by the individual bridge companies, such as the Dildine Bridge Company in Cameron. As a result, thousands of Pratts were built across the state, all very much alike in detailing, and today the Pratt truss constitutes the most populous group of through trusses. The Lost Creek Bridge is a typically configured, early example of the pin-connected Pratt through truss.

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**NAME(S) OF STRUCTURE**

Lost Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 193001.1; DeKalb County Courthouse Record, Book 4: page 264 (8 March 1894), page 275 (8 May 1894) - located at the DeKalb County Courthouse, Maysville MO; field inspection by Mitzi Rossillon, 8 September 1990.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 April 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Little Third Fork Bridge  
MHTD: 298000.1

DEKA37

**DATE(S) OF CONSTRUCTION**

1895

**LOCATION**

County Road 298 over Little Third Fork; S15, T59N, R32W  
5.2 miles northwest of Amity; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP non-eligible (score: 46)

**CONDITION**

fair

**OWNER**

DeKalb County

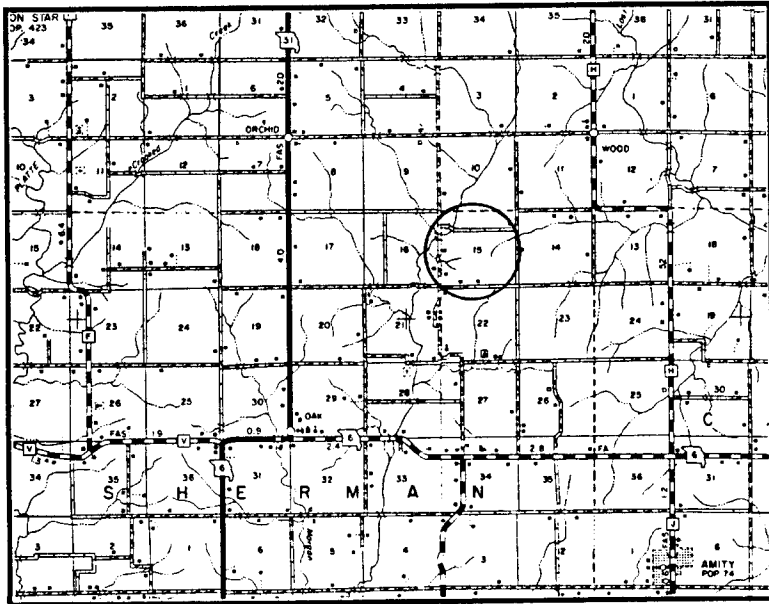
span number:	1	superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss
span length:	40.0'	substructure:	steel pile bent piers with timber wingwalls
total length:	40.0'	floor/decking:	timber deck
roadway wdt.:	11.2'	other features:	steel angle guardrails

This short-span truss crosses the Little Third Fork on an unsurfaced county road northwest of Amity. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a steel pile bent substructure. The Little Third Fork Bridge was erected in 1895 by the Dildine Bridge Company of Cameron, Missouri. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant that the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Little Third Fork Bridge in DeKalb County is a typically configured example of this mainstay truss type.

**NAME(S) OF STRUCTURE**

Little Third Fork Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 298000.1; DeKalb County Court Record, Book 4: page 332 (6 May 1895) - located at the DeKalb County Courthouse, Maysville MO..

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 April 1993



# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Little Third Fork Bridge  
MHTD: 299000.3

DEKA38

**DATE(S) OF CONSTRUCTION**

1893-94

**LOCATION**

County Road 299 over Little Third Fork; S15/16, T59N, R32W  
5.2 miles northwest of Amity; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 49)

**CONDITION**

fair

**OWNER**

DeKalb County

span number:	1	superstructure:	steel, 3-panel, pin-connected Pratt half-hip pony truss
span length:	60.0'	substructure:	steel pile bent abutments with timber wingwalls
total length:	60.0'	floor/decking:	timber deck
roadway wdt.:	11.8'	other features:	steel angle guardrails

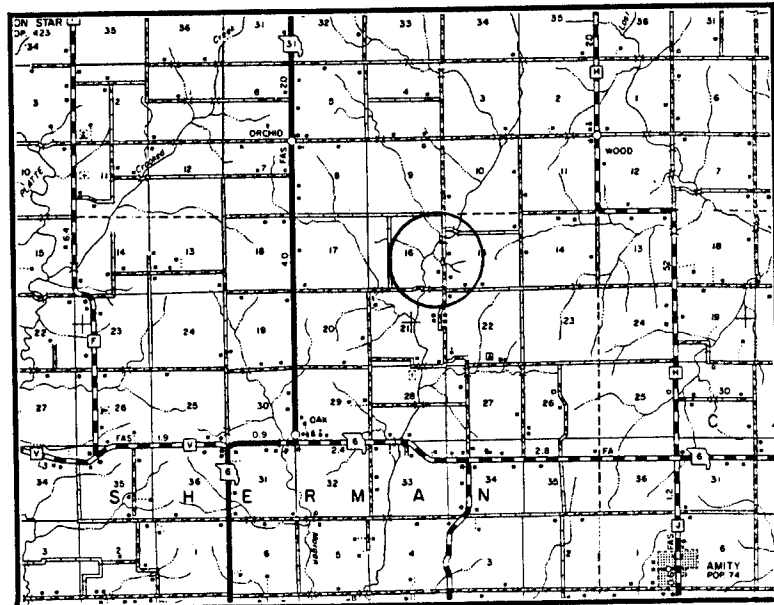
This medium-span truss spans the Little Third Fork on an unsurfaced county road northwest of Amity. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a steel pile bent substructure. The Little Third Fork Bridge was erected in 1893-94 by the Dildine Bridge Company of Cameron, Missouri. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant that the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Little Third Fork Bridge in DeKalb County is a typically configured example of this mainstay truss type. Built in 1893-94 by one of the state's most prolific bridge manufacturers, it is a noteworthy transportation-related resource.

**NAME(S) OF STRUCTURE**

Little Third Fork Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 299000.3; DeKalb County Court Record, Book 4: page 240 (6 November 1893) - located at the DeKalb County Courthouse, Maysville MO.

**INVENTORIED BY**  
Clayton Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
24 September 1994

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Jordan Creek Bridge  
MHTD: 376000.4

DEKA52

**DATE(S) OF CONSTRUCTION**

1893

**LOCATION**

County Road 376 over branch of Jordan Creek; S10, T57N, R33W  
4.0 miles southwest of Clarksdale; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 52)

**CONDITION**

fair

**OWNER**

DeKalb County

span number:	1	superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss with laced endposts
span length:	35.0'	substructure:	concrete abutments and wingwalls
total length:	35.0'	floor/decking:	timber deck
roadway wdt.:	13.4'	other features:	steel angle guardrails

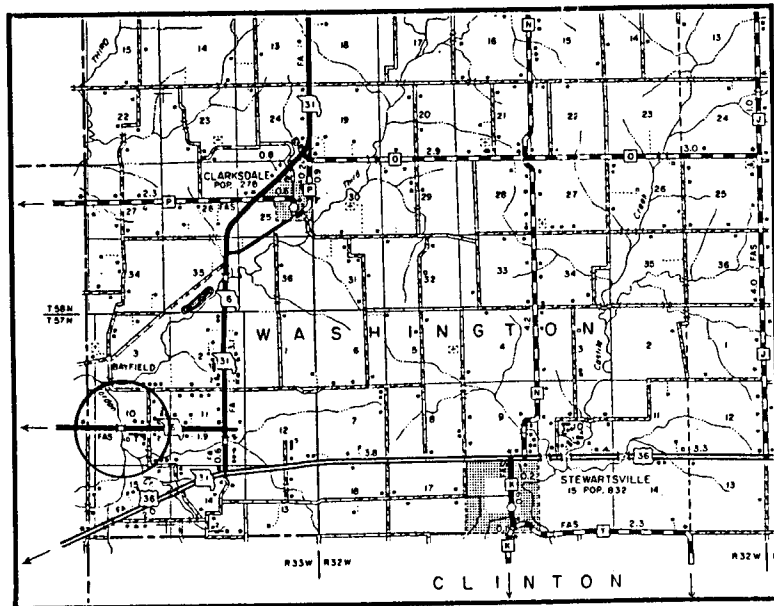
This short-span truss spans a branch of Jordan Creek on an unsurfaced county road some four miles southwest of Clarksdale. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a concrete substructure. The Jordan Creek Bridge was erected in 1893 by the Dildine Bridge Company of Cameron, Missouri. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant than the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. With its laced end posts, the Jordan Creek Bridge in DeKalb County is an atypically configured example of this mainstay truss type. Built in 1893 by one of the state's most prolific bridge manufacturers, it is a noteworthy transportation-related resource.

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**NAME(S) OF STRUCTURE**

Jordan Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION****LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP

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**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 376000.4; DeKalb County Court Record, Book 4: page 165 (26 September 1892), pages 177-78 (6 December 1892), page 227 (7 August 1893) - located at the DeKalb County Courthouse, Maysville MO.

**INVENTORIED BY**

Clayton B. Fraser

**AFFILIATION**

Fraserdesign, Loveland CO

**DATE**

6 April 1993

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# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Castile Creek Bridge  
MHTD: 391000.9

DEKA54

**DATE(S) OF CONSTRUCTION**

1895

**LOCATION**

County Road 391 over Castile Creek; S3, T57N, R32W  
3.0 miles northeast of Stewartsville; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 46)

**CONDITION**

fair

**OWNER**

DeKalb County

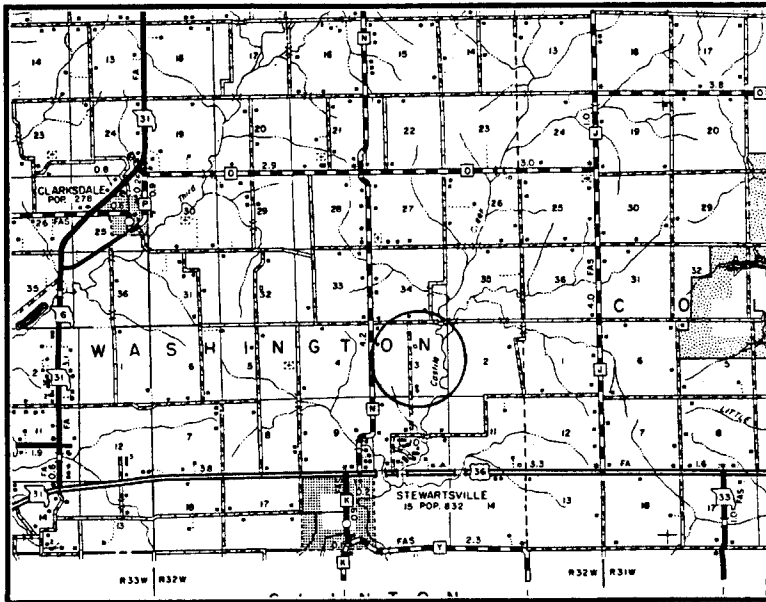
span number:	1	superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss with approach spans
span length:	40.0'	substructure:	steel pile bent pier and abutments with timber wingwalls
total length:	65.0'	floor/decking:	timber deck
roadway wdt.:	11.4'	other features:	steel angle guardrails

This short-span truss spans a branch of Castile Creek on an unsurfaced county road northeast of Stewartsville. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on steel pile bents. The Castile Creek Bridge was erected in 1895 by the Dildine Bridge Company of Cameron, Missouri, for \$357.00. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant than the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Castile Creek Bridge in DeKalb County is a typically configured example of this mainstay truss type.

**NAME(S) OF STRUCTURE**  
Castile Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.7; DeKalb County Court Record, Book 4: page 165 (26 September 1892), page 178 (6 December 1892) - located at the DeKalb County Courthouse, Maysville MO.

**INVENTORIED BY**  
Clayton Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
6 April 1993

# HAER INVENTORY

Missouri Historic Bridge Inventory

**NAME(S) OF STRUCTURE**

Castile Creek Bridge  
MHTD: 391001.7

DEKA56

**DATE(S) OF CONSTRUCTION**

1893

**LOCATION**

County Road 391 over branch of Castile Creek; S2/35, T57N, R32W  
3.4 miles northeast of Stewartville; DeKalb County, Missouri

**USE (ORIGINAL / CURRENT)**

roadway bridge / roadway bridge

**RATING** NRHP possibly eligible (score: 46)

**CONDITION**

fair

**OWNER**

DeKalb County

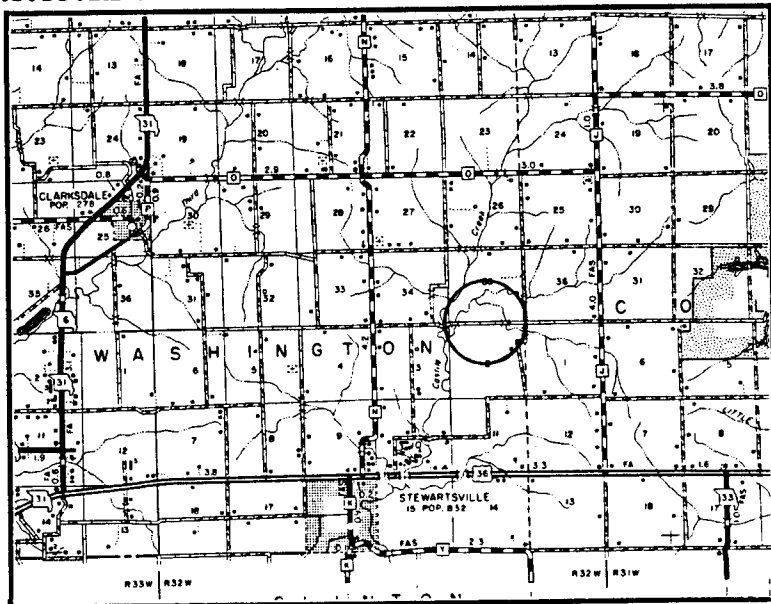
span number:	1	superstructure:	steel, 2-panel, pin-connected Pratt half-hip pony truss
span length:	30.0'	substructure:	steel pile bent abutments with timber wingwalls
total length:	30.0'	floor/decking:	timber deck
roadway wdt.:	11.3'	other features:	steel angle guardrails

This short-span truss spans a branch of Castile Creek on an unsurfaced county road northeast of Stewartville. The structure is comprised of a single pin-connected Pratt half-hip pony truss that rests on a concrete substructure. The Castile Creek Bridge was erected in 1893 by the Dildine Bridge Company of Cameron, Missouri, for \$230.00. The structure today carries vehicular traffic with its superstructure intact and its substructure replaced.

A subtype of the venerable Pratt pony truss, the Pratt half-hip configuration was marketed extensively by regional bridge companies in the early 1900s. Its primary advantage as a structural type was that, by eliminating the vertical members at the hip connection, it was more materially conservant that the standard Pratt. Its disadvantage was that it was generally limited to short-span applications: typically 30-60 feet. But Missouri's myriad small streams lent themselves to this range, and, as a result, thousands of half-hip trusses were erected across the state in the late 19th century and early 20th century. The Castile Creek Bridge in DeKalb County is a typically configured example of this mainstay truss type. Built in 1893 by one of the state's most prolific bridge manufacturers, it is a noteworthy transportation-related resource.

**NAME(S) OF STRUCTURE**  
Castille Creek Bridge

**PHOTOS AND SKETCH MAP OF LOCATION**



**LOCATION MAP**

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT  
GENERAL HIGHWAY MAP



**SOURCES**

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 391001.7; DeKalb County Court Record, Book 4: page 165 (26 September 1892), page 178 (6 December 1892) - located at the DeKalb County Courthouse, Maysville MO.

**INVENTORIED BY**  
Clayton B. Fraser

**AFFILIATION**  
Fraserdesign, Loveland CO

**DATE**  
6 April 1993